MILLER CONSTRUCTION, INC.

P.O. BOX 86 ASCUTNEY BLVD WINDSOR, VERMONT 05089-0086 **TELEPHONE (802) 674-5525 / FAX (802) 674-5245**

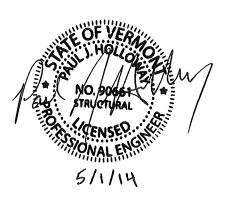
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TO: Jennif	fer Fitch, PE		DATE	PROJECT NO.
Projec	ct Manager	of Transportation	5/1/2014	Brookfield BRF FLBR (2)
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XX	WE ENCLOSE THE	FOLLOWING:	UNDER SEPARATE COVER WE AR	E SENDING THE FOLLOWING
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BY:

COFFERDAM PLAN for State of Vermont Project: Brookfield BRF FLBR (2)

Town of: Brookfield, Vermont County of: Orange



Prepared By:

Miller Construction, Inc. P.O. Box 86 Windsor, VT 05089 Tel. (802) 674-5525 Fax. (802) 674-5245

May 1, 2014

Cofferdam Plan Brookfield BRF FLBR (2)

COFFERDAM PLAN FOR State of Vermont Project: Brookfield BRF FLBR (2)

TABLE OF CONTENTS

General Project Description and Timetable

Cofferdam No. 1

Cofferdam No. 2

Attachment 1: Cofferdam Plan Views

Attachment 2: Cofferdam No. 2 Phase Drawings

Attachment 3: Design Calculations

Cofferdam Plan Brookfield BRF FLBR (2)

General Project Description and Timetable

Brookfield BRF FLBR (2) includes the replacement of Bridge No. 2 on VT RT. 65 spanning 321 feet over the body of water known as Sunset Lake in the Town of Brookfield. The project begins at a point approximately 0.13 miles west of the VT RT. 65 / Stone Road Intersection and extends southeasterly for 0.08 miles along VT RT. 65. Work will involve complete replacement of Bridge No. 2 along with related roadway and removal of the existing floating bridge superstructure, abutments, and incidental items.

Sheet pile cofferdams are required at both abutments. The cofferdam at Abutment No. 1 (Cofferdam No. 1) is designed as a Cantilever Sheet Pile Wall. The cofferdam at Abutment No. 2 (Cofferdam No. 2) is designed as a Braced Sheet Pile Wall driven to ledge.

Anticipated installation of cofferdams shall occur during May and June of 2014.

Cofferdam No. 1

Cofferdam No. 1 is designed as a Cantilevered Sheet Pile Wall in Granular Soil. Reference Material includes the USS Steel Sheet Piling Design Manual, Dated July 1984. Soil Properties are assumed. Active and Passive Earth Pressure Coefficients were determined using Log-Spiral Theory and the Simplified Method was used to develop the pressure diagram.

Required embedment depth of the sheets below the dredge line is $\mathbf{D} = 10.92$ feet. At this depth, a minimum distance of 4 feet between the centerline of battered H-Pile at Elev. 1269.5 (or 2.5 feet from the face of footing) and the line of sheeting shall be maintained to eliminate conflicts with H-Pile. Required Section Modulus is very low. All sheet sections on hand meet the requirement.

Although internal bracing is not required, the driving frame may be left in place for convenience.

Cofferdam No. 1 is subject to field revisions based on actual conditions. Any changes shall be documented and this plan shall be amended.

Cofferdam No. 2

Cofferdam No. 2 is designed as a Braced Sheet Pile Wall in Loose Sand. Reference Material includes the USS Steel Sheet Piling Design Manual, Dated July 1984. Soil Properties are assumed. Active Earth Pressure Coefficient was determined using Log-Spiral Theory, and the pressure diagram was developed using the Modified Method for Braced Cuts in Sand, After Teng.

Cofferdam Plan Brookfield BRF FLBR (2)

Cofferdam No. 2 shall be installed in 3 separate phases.

Phase 1:

Phase 1 includes the driving of sheets to ledge, the cofferdam excavation, and the internal bracing. The distance between the top of ledge and the centerline of the bottom waler shall not exceed 3 feet.

The distance between additional internal bracing shall not exceed 20 feet and shall be oriented with the web horizontal.

Phase 2:

Phase 2 includes the pinning of sheets to the ledge where the distance between the top of ledge and the centerline of the bottom waler needs to exceed 3 feet. After pinning, the bottom waler and subsequent bracing shall be adjusted.

Where required, each individual sheet shall be pinned to the ledge using a piece of #8 Rebar with a minimum embedment depth of 10 inches. Rebar shall be grouted into the ledge (product to be determined based on actual conditions). Total pin length shall be a minimum of 16 inches.

After pinning of the sheets is complete, the bottom waler and subsequent internal bracing shall be adjusted to a higher elevation to maintain a horizontal orientation. The distance between the pins and the centerline of the bottom waler shall not exceed 5 feet.

Phase 3:

Phase 3 includes the placement of the bottom lift of Class C Concrete for Subfooting and the removal of the bottom waler where conditions allow.

The lift of Class C Concrete shall be placed to an elevation below the bottom waler and shall receive a raked finish. An additional lift of Class C Concrete will be required to reach the required elevation per contract.

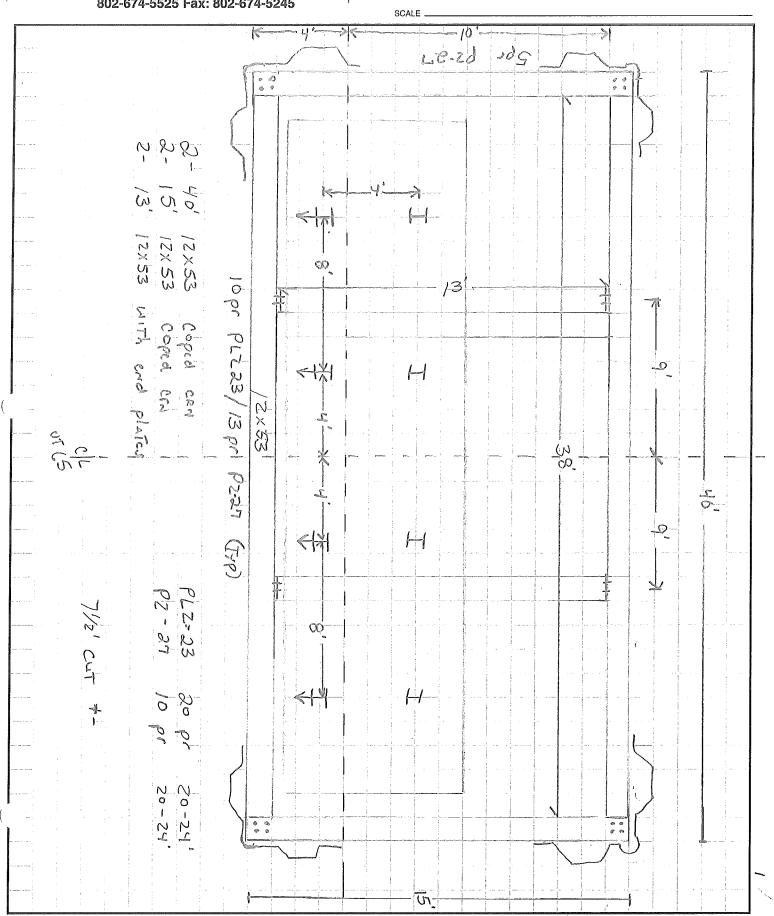
If conditions allow the bottom of all sheeting to bear laterally on a minimum thickness of 6 inches of concrete, then the bottom waler and subsequent bracing shall be removed. If conditions do not allow 6 inches of bearing on concrete, then the bottom waler and subsequent bracing shall be adjusted to a higher elevation. The distance between the top of concrete and the centerline of the bottom waler shall not exceed 5 feet.

Required Section Modulus is very low. All sheet sections on hand meet the requirement.

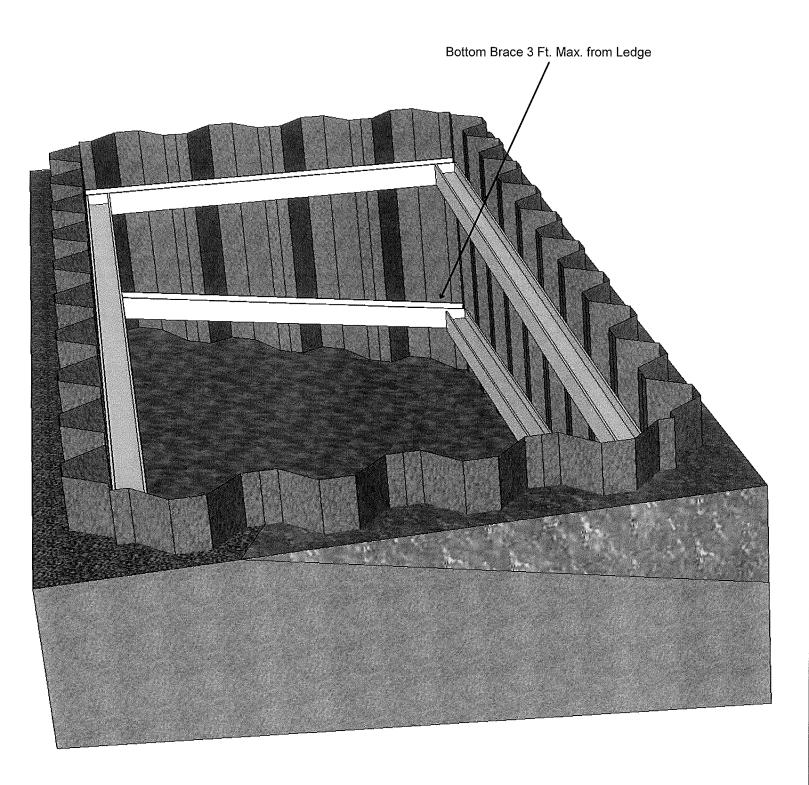
Cofferdam No. 2 is subject to field revisions based on actual conditions and ledge elevations. Any changes shall be documented and this plan shall be amended.

Miller Construction, Inc. 3103 US Route 5 South P.O. Box 86 Windsor, Vermont 05089 802-674-5525 Fax: 802-674-5245

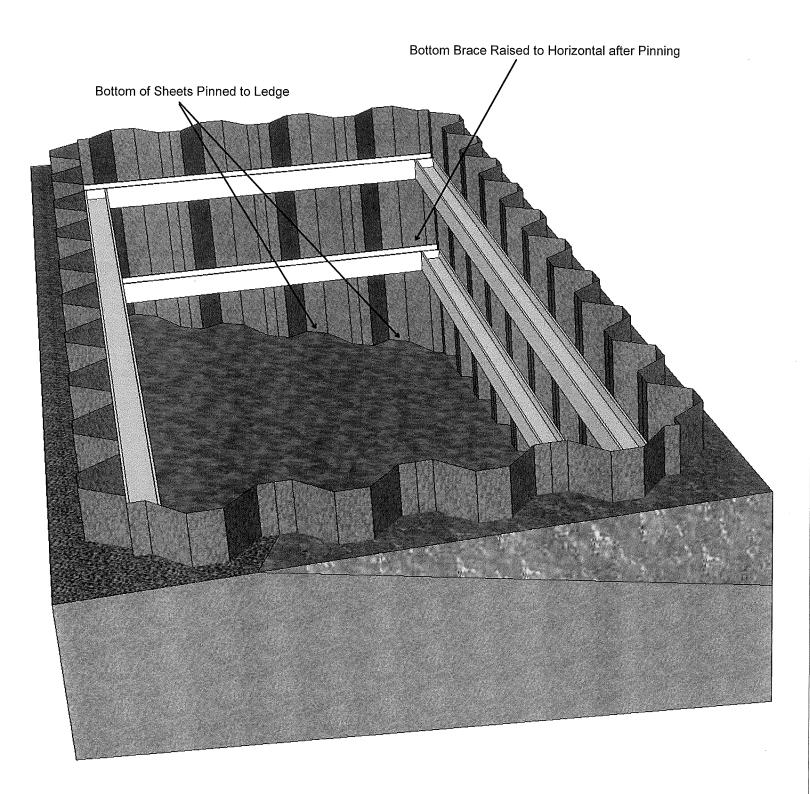
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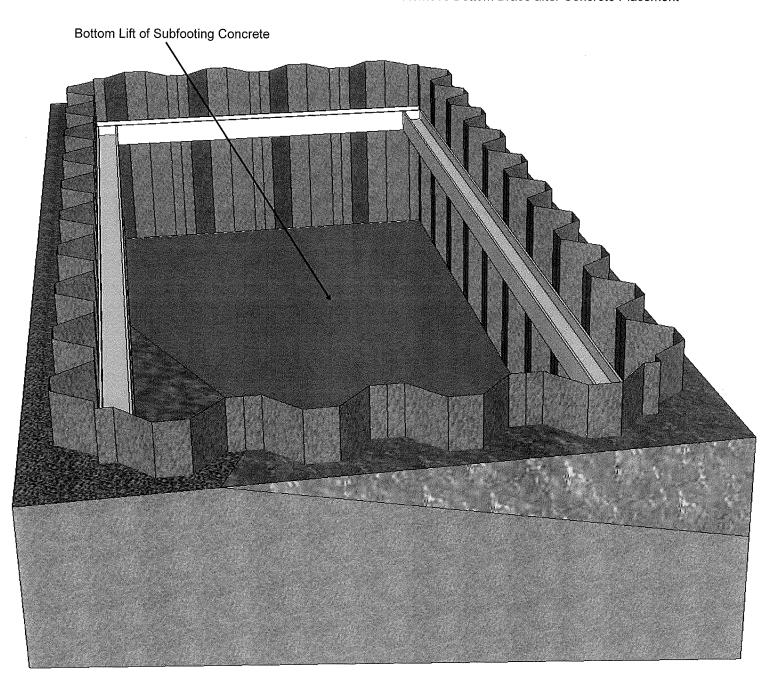
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^{*}Internal Bracing not Shown for Clarity



Remove Bottom Brace after Concrete Placement



^{*}Internal Bracing not Shown for Clarity

Miller Construction, Inc.

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